Proposed Part 8 Residential Development,
Balally, Sandyford, Dublin 16

Architect's Design Report

Dún Laoghaire-Rathdown County Council
August 2024

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Contents

1.0 Introduction

Image 1: Site Location identified within a wider context

This design report was prepared for the National Development Finance Agency (NDFA) on behalf of Dún Laoghaire-Rathdown County Council, to accompany a Part 8 proposal for the development of 62 no. residential units on a site of circa 0.3216ha hectares in area, located at Balally, Sandyford, Dublin 16.

The project is part of Social Housing Bundles 4 & 5 which is a social housing PPP programme being delivered by the National Development Finance Agency (NDFA), in conjunction with the Department of Housing, Local Government and Heritage and relevant local authorities.

A PPP is an arrangement between a public authority and a private partner designed to deliver a public infrastructure project or service under a long-term contract. In the case of this PPP project, the private partner will finance and construct the homes, then provide maintenance and tenancy management services for a 25 year service period with the homes maintained to a pre-defined standard. The homes remain in local authority ownership.

To date the Social Housing Bundle programme includes*:

- Bundle 1 - 534 homes were delivered across six sites in Dublin, Kildare, Louth and Wicklow in 2020 and 2021
- Bundle 2 - 465 homes were delivered across eight sites in Cork, Clare, Galway, Kildare, Roscommon and Waterford in 2021
- Bundle 3 - 486 new homes will be delivered across six sites in Dublin, Kildare, Sligo and Wicklow with planning completed on all sites. The tender process commenced in Q4 2022
- **Bundles 4 and 5 - circa 1,900 new homes will be delivered across 18 sites in Dublin, Kildare, Louth and Wicklow. Design development underway**
- Bundle 6 - circa 500 new homes across seven sites in Cork, Kildare and Wicklow
- Bundle 7 - circa 600 new homes across six sites in Dublin, Galway, Limerick and Wexford

The development proposed in this application is one of the 18 sites contained in Bundle 4 and 5.

The PPP model strives to deliver efficiency by grouping (bundling) projects together to create economies of scale. The projects in each bundle are developed in parallel and a key consideration at design stage is to deliver levels of standardisation and commonality across the schemes to help with delivery while also responding to the immediate context , constraints and opportunities of the individual sites.

This design report was prepared on behalf of -

- The National Development Finance Agency (NDFA) and
- Dún Laoghaire-Rathdown County Council,

to accompany a Part 8 proposal for the development of 62 no. residential units on a site of circa 0.3216ha hectares in area, located at Balally, Sandyford, Dublin 16.

The proposed development includes:

1. 62 no. apartment units in a 5-6 storey building over undercroft area, including 31 no. one bed units; 21 no. two bed units; and 10 no. three bed units;
2. 1 no. community facility of 249m² at upper ground level.
3. Energy Centre at sixth floor level and an external plant area set back at fifth floor roof level.
4. Undercroft area at lower ground level comprising (a) 1 no. ESB substation (b) car and bicycle parking; (c) bin storage; (d) bulk storage area; and (e) supporting mechanical, electrical and water infrastructure.
5. Landscaping works including provision of (a) communal open space; (b) new pedestrian and cycle connections linking Blackthorn Dive with Cedar Road; and (c) public realm area fronting onto Blackthorn Drive.
6. All associated site development works including (a) vehicular access off Cedar Road; (b) pedestrian and cycle access off Blackthorn Drive; (c) public lighting; (d) varied site boundary treatment; and (e) temporary construction signage.

This report has been prepared by MCORM Architecture and Urban Design to describe the architectural design of the proposed development, which has been developed in collaboration with the multi-disciplinary project team.

This Design Statement has been prepared by MCORM Architecture and Urban Design to describe the architectural design of the proposed development, which has been developed in collaboration with the multi-disciplinary project team.

Development Agency	- N.D.F.A
Project Manager	- Turner Townsend
Planning Consultants	- HRA
Architects	- MCORM Architecture and Urban Design
Civil Engineers	- Malone O'Regan
M&E	- Semple McKillop
Landscape	- Mitchell + Associates
Ecology	- NM Ecology
Arborist	- CMK Horticulture

2.0 Site Context

Image 2: Aerial view of Balally Site

Image 3: Street View image of Site from Blackthorn Drive looking north west. Scout Hall to the right in the disatnce north of the site

2.1 Site Description

The subject site, measuring approximately 0.3216ha, is greenfield in nature and is bound by Cedar Road to the north, Balally Shopping Centre to the west, Blackthorn Drive to the south and open space to the east.

It currently forms part of a large open space between Balally Shopping Centre and Drummartin Link Road, and is fully accessible to the public.

Vehicular access is possible its northern end from Cedar Road which has access points into the adjacent shopping centre loading yard and scout hall. While vehicular access is not feasible from Backthorn Drive to the south due to its proximity to a major road junction on nearby Drummartin llnk Road, it is fully accessible for pedestrians

The site is fully serviced by existing drainage and watermains infrastructure as it is located in an existing suburban area

The site is separated from existing residential areas to the south by Blackthorn Drive, and to the north by Cedar Road and a further pocket of open space. A church and primary school are located just west of the adjoining shopping centre.

The site is Zoned Objective NC "To protect, provide for and-or improve mixed-use neighbourhood centre facilities" in the **Dún Laoghaire-Rathdown County Development Plan** 2022-2028. A social housing scheme with community room facility is proposed to complement the adjoining mix of uses in the shopping centre in accordance with the zoning.

2.2 Brief

The brief for this site, as advised by Dún Laoghaire-Rathdown County (DLRCC), is a development of 62 no. residential units as follows:

- 31 no. 1 bed units
- 32no. 2 bed units
- 10 no. 3 bed units

plus a community room.

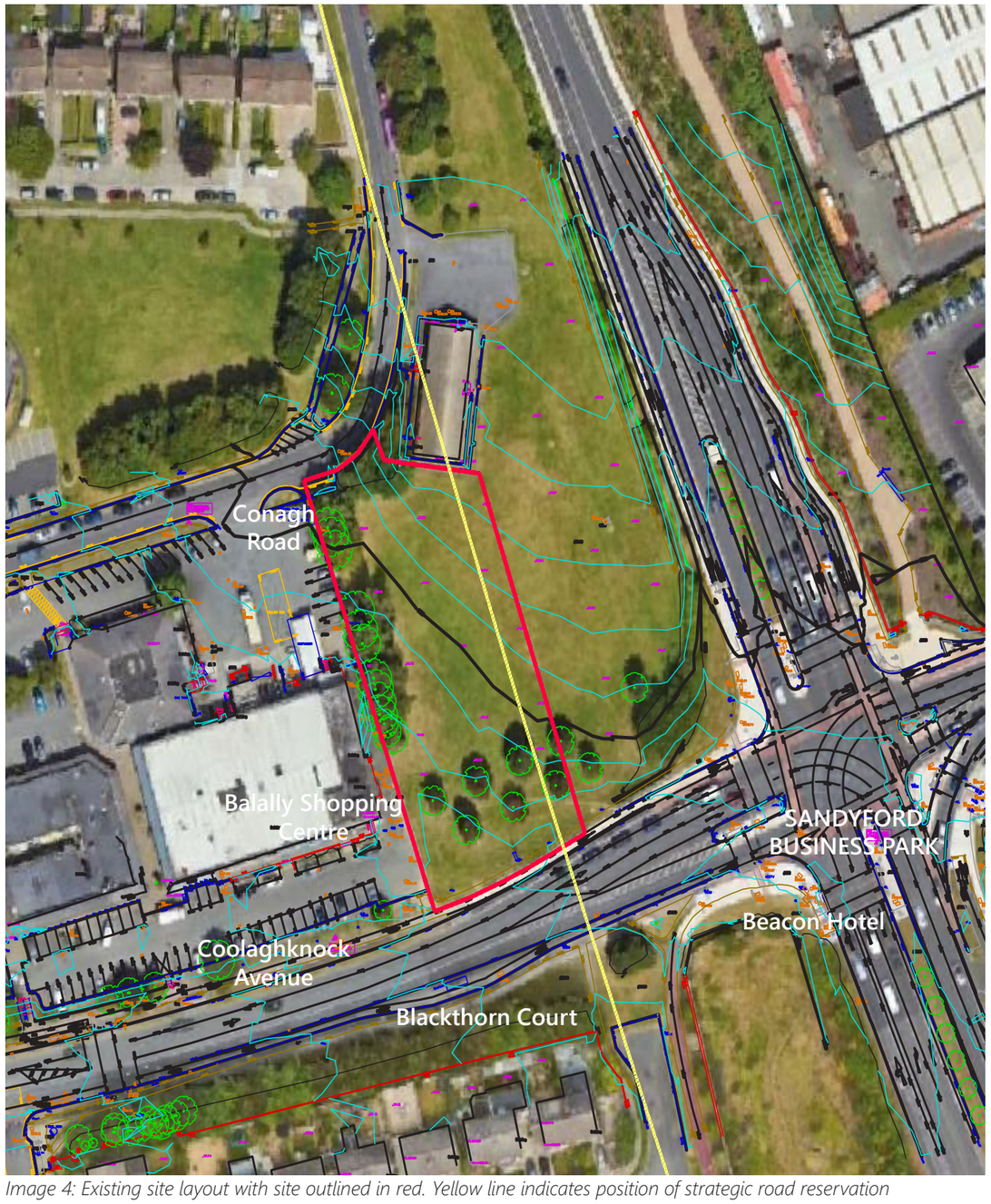

Image 4: Existing site layout with site outlined in red. Yellow line indicates position of strategic road reservation

2.3 Site Constraints

The site is fully permeable for pedestrian access along its southern, eastern and northern interfaces, as it currently forms part of a larger area of open space that is accessible to the public.

Its western interface forms the boundary to the Balally Shopping Centre lands. Vehicular access is feasible at its northern point from Cedar Road adjacent to the existing Scout Hall.

The site is not currently identified as having any flood risk according to CFRAM mapping for the area.

There are a number of existing trees on the site, most of which are located along its western boundary with the shopping centre site and to the north adjacent to the Scout Hall comprising various species, in various conditions.

The eastern side of the site currently forms the edge of a long-term strategic road reservation, the original intention of which was to facilitate the provision of a road route from the M50 to Dublin port further north, as identified in the current Dún Laoghaire-Rathdown County Development Plan 2022-2028.

There is a fall across the site from southwest to northeast, from a high point of c. 96.95 to c. 93.0, a differential of approximately 3.7 metres.

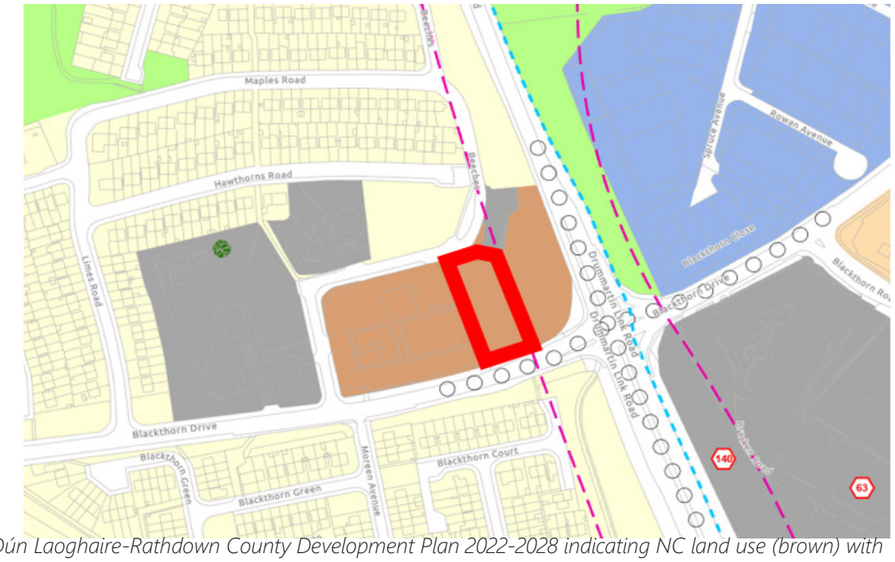

Image 5: Extract from Dún Laoghaire-Rathdown County Development Plan 2022-2028 indicating NC land use (brown) with strategic road reservation in purple dashed line

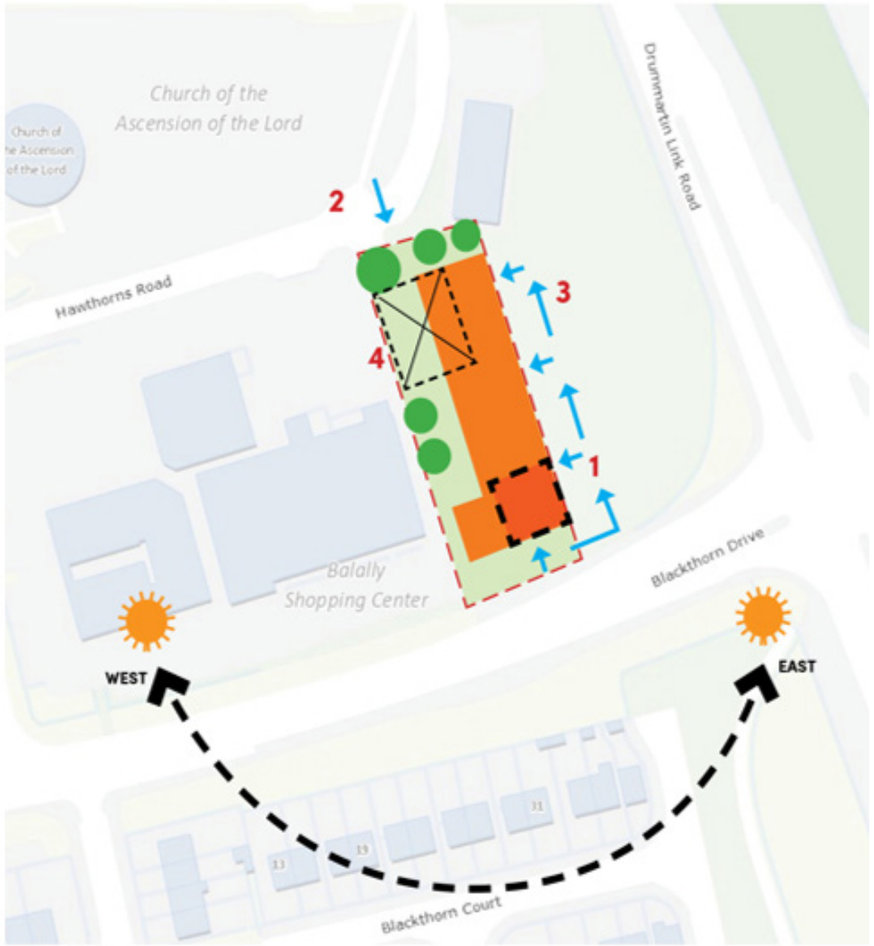

Image 6: Proposed site strategy

2.4 Site Design Approach

The development lands approximately 91 metres in length and approximately 36 metres in width defining a compact infill site. The layout strategy responds to its immediate context of the adjoining developments, several pre-planning discussions with DLR and requirements of the county development plan.

A linear block is laid out along the length of the site to provide a strong and legible urban edge to the open space to the east. Emphasis in mass and elevational treatment is identified at its southeastern corner facing onto Blackthorn Drive, and more significantly facing the large intersection further east at Drummartin Link Road (1).

It is envisaged that vehicular access will be facilitated at the northern end of the site from Cedar Road. This is also the lowest point of the site providing discreet access into an undercroft at this lower ground level (2)

The main pedestrian route runs along the eastern facade of the block with the main points of access from this route, encouraging active frontages to this elevation (3).

The block configuration defines public and private, with its private realm running along the western side of the building (4).

This north-south configuration is optimal for good aspect to the apartment units, be they single or dual aspect.

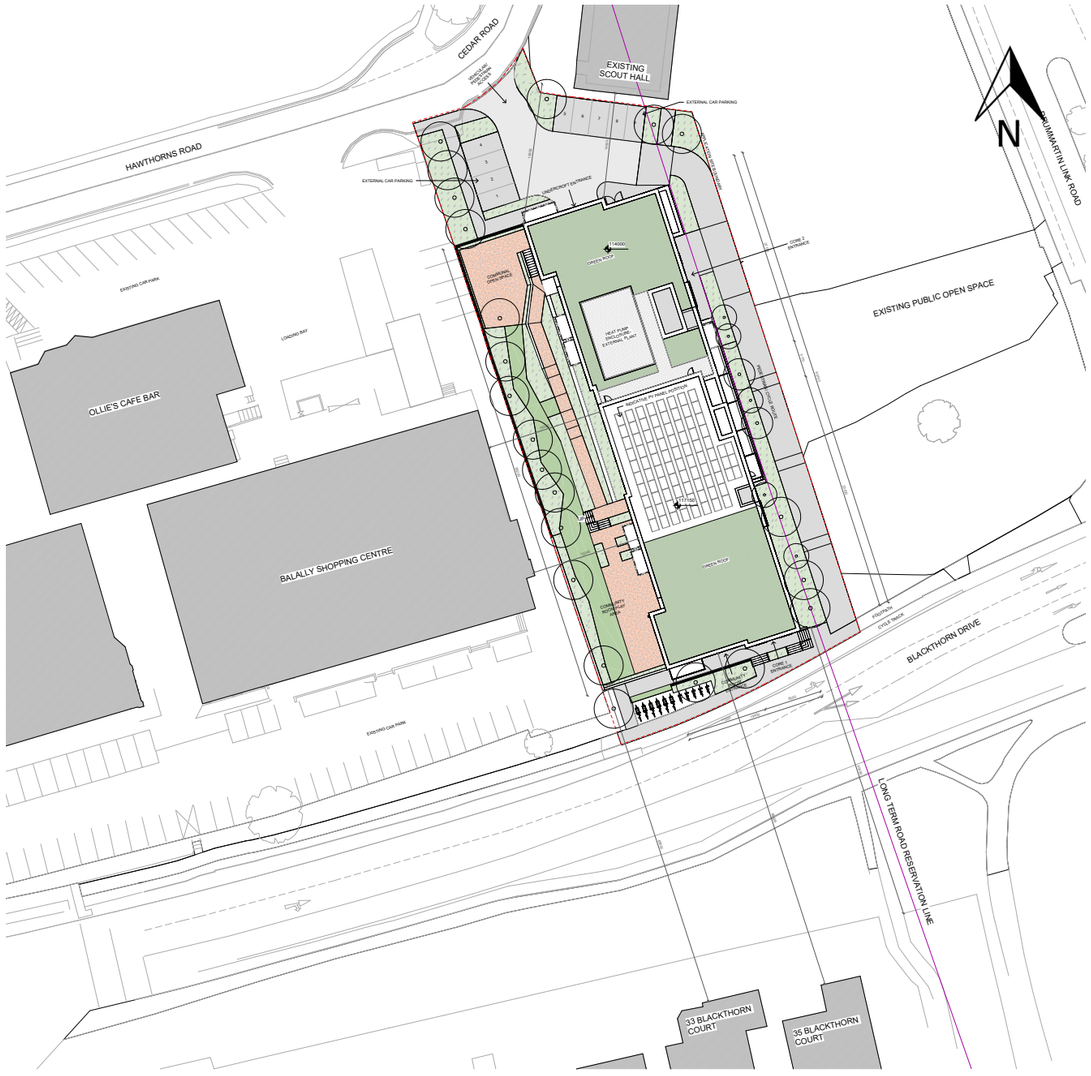

Image 7: Proposed site layout

3.0 Urban Design Rationale/ Design Criteria

The design rationale outlined below identifies the key issues considered in the design process for the proposed residential scheme on the site under the 12 criteria set out in the Urban Design Manual – A Best Practice Guide 2009.

"At the scale of the neighbourhood"

3.1 Context: How does the development respond to its surroundings?

A six-storey linear block is proposed for the site that responds to the evolving urban character of this area, particularly along Blackthorn Drive adjacent to Sandyford Business District.

The southern end of the block, given its prominence is anchored in new public realm that warps around the corner at the entrance to the community room facility at ground level. This public realm continues along the eastern flank of the block providing a edge to the scheme facing the open space. This provides access to entrances into the residential component of the scheme.

The northern end of the block provides for discreet vehicular and service access (parking, refuse collection/ ESB substation, away from the prominent "front of house" community use.

The block is positioned along eastern side of the plot, with resulting open space along its western flank providing a buffer between it and th shopping centre site adjacent. This area becomes the "rear garden" of the scheme, providing communal amenity space for the residents.

3.2 Connections: How well connected is the new neighbourhood

The site is located at a prominent position in Balally, located along Blackthorn Drive which runs from the Drummartin Link Road in the east, connecting Sandyford Business District and the M50 to Sandyford Road in the west connecting with established residential neighbourhoods in Balally, Dundrum and the historic village of Sandyford.

The site will create a new north-south route running from Blackthorn Drive up to Cedar Road for the benefit of pedestrians and cyclists. This route will feed into and contribute the wider network of cycle routes in the area.

Residents will be served by existing high-frequency bus services (S8 Dun Laoghaire- Sandyford- Tallaght) as well as nearby luas Green Line stop (Kilmacud).

Image 8: Southern Elevation to Blackthorn Drive- Community Room and Residential frontage at ground floor

Image 9 3D MassingView at the southeastern corner of the scheme

3.3 Inclusivity: How easily can people use and access the development

The block is laid out to be legible to residents, users and visitors, with the entry points into the apartments via 2 main entrances along the pedestrian route running along eastern facade of the block. The community room is entered at its most prominent south facing facade onto Blackthorn Drive. Despite the challenges presented by the sloping topography of the site, all the entrances have been designed to be fully accessible.

3.4 Variety: How does the development promote a good mix of activities?

The proposed development is situated on NC (Neighbourhood Centre) zoned lands like the adjacent Balally Shopping Centre. The development consists of a mix of mainly residential with a ground floor community room both of which will complement the existing mix of uses in the shopping centre.

"At the scale of the site"

3.5 Efficiency: How does the development make appropriate use of resources , including land?

At 202.7 units per hectare, the development represents an efficient use of this compact site in accordance with development plan and national density requirements, and when considered in relation to the site context and surrounding neighbourhood. 50% of the apartments have dual aspect with the other 50% having either south, east, or west aspect. The building form is simple and consists of 12 dwellings per floor on a typical level. With this simple form, the building has an efficient external building envelope and therefore an efficient thermal envelope.

3.6 Distinctiveness: How do the proposals create a sense of place?

The scheme consists of a single block, incorporating 62 apartments and a community room at ground floor in a 6-storey block that steps up in line with the sloping topography of the site. This creates a hierarchy within the block, with the portion housing the community space reading higher and more prominently onto Blackthorn Drive. A framed device is introduced here to add emphasis at the south eastern corner, which signifies the community use location as well as a landmark at this corner which will be highly visible from the Drummartin Link Road intersection further east.

3.7 Layout: how does the proposals create people friendly streets and spaces?

The block is designed to run along the eastern edge of the site. This helps create both highly supervised public realm around the south, east and north facades with defensible space at the rear (west) which becomes a compact communal garden for residents. Continuous active building lines are provided to the public realm. The compact urban form proposed seeks to eliminate gaps and breaks in the streetscape. This results in public spaces that has strong passive surveillance in all areas. Five floors of balconies to all facades balconies overlooking public spaces further enhance that sense of security.

Image 10 View looking West from Intersection at Drummartin k Road along Blackthorn Drive

A new pedestrian/ cycle route runs along the eastern edge/ facade, with vehicular traffic being confined to the northern access to the parking spaces. This route provides the main access to the apartments as well as the secure cycle storage area at lower ground level. Minimal parking spaces are located discreetly to the north of the block (10 no.) with a further 15 no. within the undercroft out of sight.

3.8 Public Realm: how safe, secure and enjoyable are the public areas?

The site layout affords high levels of passive surveillance and overlooking on all facades, in particular onto the public realm on the southern, eastern and northern facades. The public realm also minimises vehicular traffic, which is confined to a small area to the northern entrance from Cedar Road. The eastern route is intended for pedestrian and cycle through traffic only.

A mini-plaza is located to the south onto Blackthorn Drive, providing a quality space at the forecourt to the residential and community room entrance, with the provision of cycle stands and planting.

Image 11: East facing elevation to public Open Space. Maximum passie supervision is provided as well as active ground floor

Image 12: View looking south west from existing public space. Scout Hall to the right

Image 13: View looking west from Drummartin Link Road

"At the scale of the home"

3.9 Adaptability: How will the buildings cope with change?

All proposed residential units are designed to be readily accessed, without the need for altering or adapting, by people with disabilities. This builds in an automatic adaptability providing for the changing needs of occupants as they age or become impaired, without the need for adjusting building approaches, entrances or internal circulation. The design of the proposed buildings is in accordance with Part M of the Buildings Regulations, fostering an inclusive approach to the design of the built environment. Provisions in each apartment building include level access entrances, suitably designed lift and stairs and accessible rooms and sanitary facilities.
2-bedroom apartment type "B4P-5" (11 no.) is designed to be universally accessible.
It is envisaged that the internal walls of individual units will be mostly non-load bearing providing for easy future re-configuration making the apartments individually adaptable for the changing needs of occupants.
All units shall be A-rated which provide energy robustness and efficiency for future residents. The development is also designed in accordance with best practice sustainable drainage measures including suitable provision for future climate change.

3.10 Privacy and Amenity: How does the scheme provide a decent standard of amenity?

Quality Landscaping is provided within the courtyard communal garden and immediately outside at ground level, and will incorporate extensive planting, seating and play areas with easily accessible ancillary facilities such as parking, refuse storage and plant in the undercroft.

3.11 Parking: How will the parking be secure and attractive?

Car Parking (24 no.) is provided at a rate of 0.387 space per dwelling, in line with the parking zone area (No. 2 in the county, given its proximity to quality public transport. 9 spaces are located to the north of the block, with the remaining 15 spaces in the undercroft area. 2 no. disabled spaces are located in the undercroft in close proximity to the lift cores. Secure bicycle parking is provided in the undercroft, with a dedicated access point from the east facade connecting to the pedestrian/ cycle route, thus avoiding any conflict with vehicular traffic in the undercroft.

3.12 Detailed Design: How well thought through is the building and landscape design

A consistent materiality is created across the building. 2 contrasting brick types are used to emphasises the different planes of the block as well as the plinth running around its base. A lighter coloured brick is applied to the tallest and most prominent element at the southeast corner, with the darker brick applied to th recessed planes.

Note: Appendix B of this report also contains a tabulated response to the items listed in Appendix D - Design Checklist (Key Indicators of Quality Urban Design and Placemaking) of the Sustainable Residential Development and Compact Settlements Guidelines for Planning Applications.

Image 14: Landscape Plan

4.0 Landscape Design Strategy

The landscaping design is fully integrated with the block design and responds to the various site conditions on each of the facades.

The southern facade presents a more civic character to Blackthorn Drive, a this marks the main point of arrival into the development. This area consists of a stepped and sloped access to the entrance of both the community room and the southern entrance core to the apartment block. This area combines hard landscaping, planting bays and accommodates external cycle parking areas.

The palette of paving materials continue around to the eastern facade of the block into the pedestrian and cycle route, running along the sloping topography beside the building. This provides access to the second residential core along the facade as well as a dedicated access to the secure cycle parking in the undercroft.

This route also provides service access to the refuse store and substation at the north east corner of the building. The route provides a definition to the eastern edge of the development against the public open space.

Replacement tree planting is proposed where existing trees cannot be retained due to the provision of vehicular access at the northern end of the site, and along the western boundary between the scheme and shopping centre.

A communal garden is located discreetly along the western flank of the building separated from the external parking by a retaining wall and railing and is fully and directly accessible to all residents from both cores. A small play area is located here (85m^2) for the use of toddlers up to the age of 6.

Resting areas are provided along a compacted gravel footpath in the communal garden, with ground-cover planting, woodland cover planting and pollinator friendly planting at appropriate locations.

A wall and railing separate the communal garden from the civic area at the front of the block

A full landscape plan and report has been prepared by Mitchell and Associates and is included with the application.

5.0 Community Space

A Community Space is provided as part of the development.

It is 249m² and is intended as flexible space for the use of community groups.

It is located at the upper ground level of the block facing Blackthorn Drive and its prominent location makes it convenient for accessing the facility by foot or bicycle. Cycle parking is located directly to the entrance.

The internal space can be subdivided as required by the end user, subject to future design and fit out. The layout is served by a generous provision of fenestration, ensuring that this space provides the maximum animation at ground level as well as good supervision of the outdoor spaces.

16 No. short stay bicycle spaces have also been provided externally in close proximity to both the community space and adjacent main entrance to the apartment core. This is to serve visitors to both the apartments and community space.

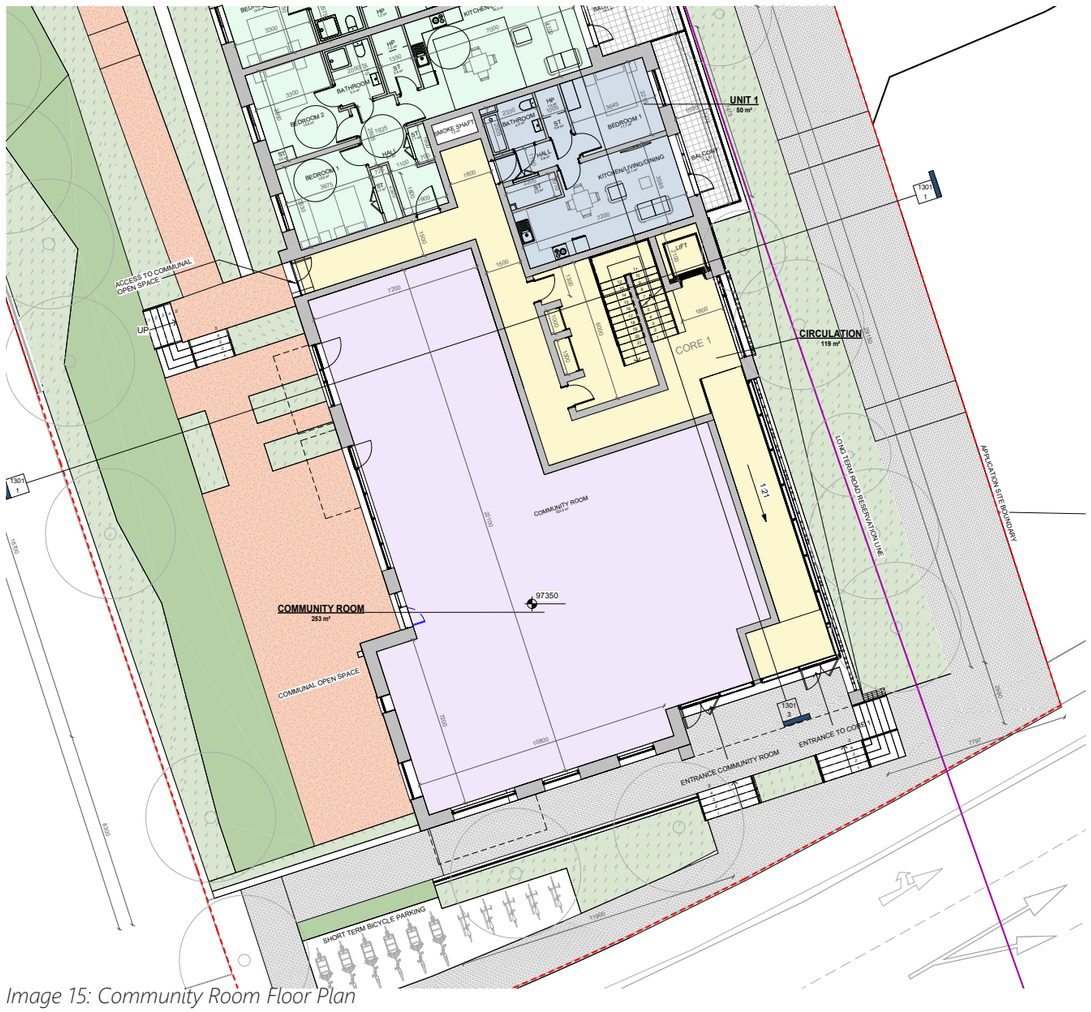

Image 15: Community Room Floor Plan

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6.0 Materials and Finishes

Images 16/17: Proposed primary materials of light coloured brick (left), and a constrasting darker brick (right). Refer to elevation drawings for details of other materials.

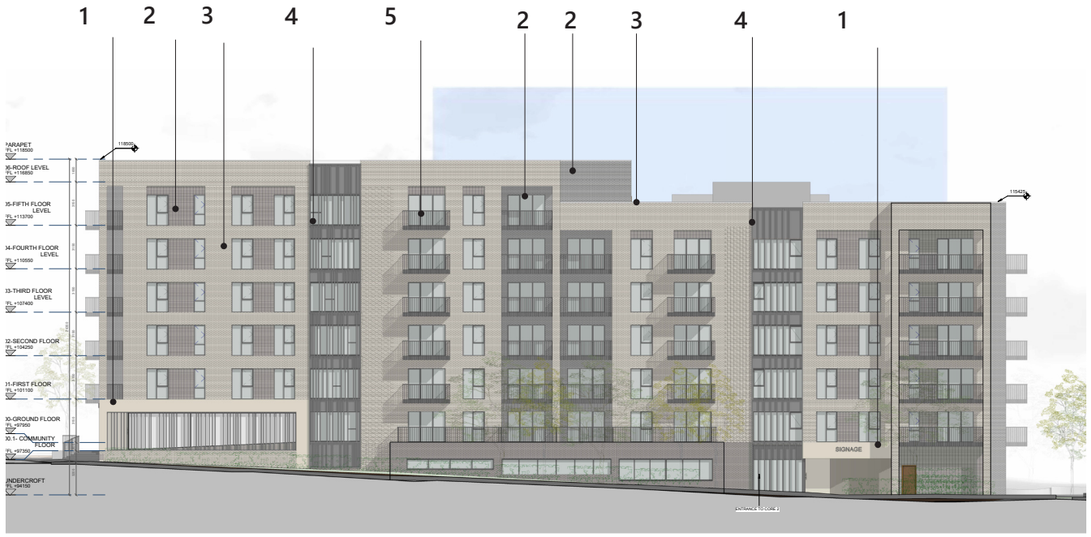


Image 18: East elevation facing public open space

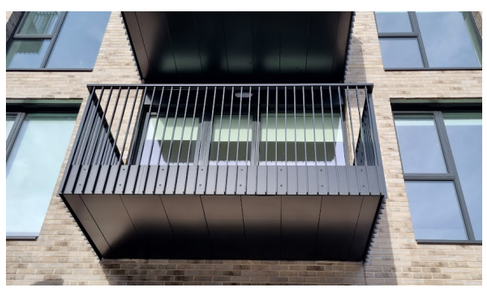


Image 19: Reference image of a similar balcony

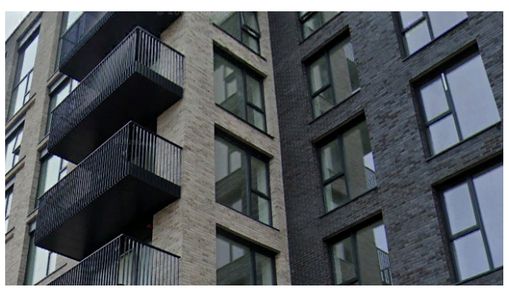


Image 20: Reference image of contrasting brick and metal balconies

A restrained and coherent palette of materials is proposed to create a simple and modern aesthetic for the new development

A light buff/grey clay brick with white mortar is proposed as the field colour for the scheme.

A darker brick provides contrast and is applied to recessed planes or where emphasis is placed on a change of surface to express the form of the block. It also acts as a plinth to the block at lower ground level, at the entrance to the northern residential core, refuse storage, sub station and car park undercroft access. A frame of natural stone cladding is wrapped around the community and residential frontage at upper ground level to express it prominence and to reflect the traditional shopfront concept.

The window frames, balcony railings, fascia, soffit, verges, associated rainwater goods and flat free draining metal clad entrance canopies are proposed in a single unifying colour maintaining a consistent simple colour palette across the scheme.

On street parking paved areas are to be finished in permeable to paving to provided increased natural SUDs features within the site.

1. Natural stone frame to facade
2. Dark brick contrasting panel
3. Light buff/ grey brick
4. Coloured panels to glazing system. Colour to match metal goods throughout
5. Metal balcony- colour to match metal goods throughout

7.0 Schedule of Accomodation

The proposed brief includes a broad mix of typologies, and this variety should assist in forming a balanced, sustainable community. A summary breakdown of the site statistics is included below:

KEY SITE FIGURES

	m²		ha	
Application Site Area (As per Red Line Boundary)	3216	m²	0.3216	ha
Plot Ratio Gross (Redline boundary/ Gross Floor Area)	0.552		0.552	
Residential Density (See Note 1 below)			201.40	d/ha
Site Coverage Gross (1248m² / 3216m²) (See Note 2 below)	38.81%		38.81%	
Dual Aspect Ratio 31:62				
Dual Aspect %	50.00%		50.00%	
Public Open Space Provision	202	m²	0.0202	ha
Communal Open Space Provision (See Note 3 below)	638	m²	0.0638	ha
Private Amenity Space Provision per dwelling (Refer to HQA- Schedule of Accommodation)				

Note 1- Residential Density Calculation

Basis for Calculation-

Site Area	3216	m²
Overall Gross Floor Area (Excluding Plant Areas, Refuse Storage, Undercroft Car Parking, Undercroft Bicycle Parking)	5823.9	m²
Overall Non-residential Gross Floor Area (i.e. Creche)	#REF!	m²
Number of Residential Units	62	no.

Calculation (Methodology from Appendix B: Measuring Residential Density; Sustainable and Compact Settlements Guidelines for Local Authorities 2024-

Residential Gross Floor Area as a portion of Development						95.72%	
Site Area for Density Purposes		0.3216	ha	x	95.72%	0.3079	ha
Residential Density		62	no. dwellings	/	0.3079	**201.40**	**d/ha**

Note 2- Basis of Site Coverage Calculation

Footprint of Development	1248	m²	0.1248 ha

Note 3- Dual Aspect Units	Dual Aspect Units		Overall Units		%
No. of Dual Aspect Units	31	no.	62	no.	50.00%

Note 3- Communal Open Space Provided	**638**	**m²**	**0.0638**	**ha**
Communal Open Space Required	**392**	**m²**	**0.0392**	**ha**

Based on-	1 BED	2 BED	2 BED	TOTAL
No. of units	31	21	10	
multiplied by (m²/ unit)	5	7	9	
Total	155	147	90	**392 m²**

7.0 Schedule of Accomodation (Continued)

GROSS FLOOR AREA OF DEVELOPMENT

	RESIDENTIAL		COMMUNITY ROOM		TOTAL	
Lower Ground Gross Internal	84.6	m²			84.6	m²
Upper Ground Gross Internal	792.0	m²	249.0	m²	1041	m²
Level 01 Gross Internal	1041	m²			1041	m²
Level 02 Gross Internal	1041	m²			1041	m²
Level 03 Gross Internal	1041	m²			1041	m²
Level 04 Gross Internal	1041	m²			1041	m²
Level 05 Gross Internal	534.3	m²			534.3	m²
TOTAL	5574.9	m²	249	m²	**5823.9**	**m²**

Ancillary Uses (Excluded from Gross Floor Area)

Undercroft Car Park	488	m²
Undercroft Bicycle Storage Area	211	m²
Refuse Store, incl. lobby	17.9	m²
Sprinkler Plantroom	54.8	m²
OR/ UPS Room	15	m²
Cold Water Storage- including for Sprinklers	151.6	m²
Bulk Storage	28.6	m²
Meter Room 1	12.5	m²
Meter Room 2	15	m²
Comms Room 1	15.2	m²
Comms Room 2	15.6	m²
ESB Substation (Including Switch Room)	24.5	m²
Energy Centre Level 5	10.5	m²
TOTAL	**1060.2**	**m²**

COMMUNITY ROOM	**249**	**m²**

CAR PARKING PROVISION

Surface External Spaces- (Residential)	9
Undercroft Spaces- (Residential)	15
Total	**24**

BICYCLE PARKING PROVISION

Location of Spaces-

Undercroft (Secure Long Stay- for Residents)	108	Basis- Min 1 long stay space per bedroom* (103 min. required)
Undercroft (Secure Long Stay- for Community Room Staff)	2	Basis- based on Other Uses Not in Table- Minimum 2**
Surface (Short Stay Visitor for Community Room and Residential)	16	Basis- 1 short stay per 5 dwellings***/ Minimum 2 spaces, non-residential****
Total	**126**	

*In accordance with SPRR 4 - Cycle and Storage, Sustainable Residential Development and Compact Settlements Guidelines for Local Authorities 2024

**In accordance with Table 4.2 Cycle Parking for Residential Development, Standards for Cycle Parking and associated Cycling Facilities for New Developments; Jan 2018; DLRCC

***In accordance with Table 4.1 Cycle Parking for Residential Development, Standards for Cycle Parking and associated Cycling Facilities for New Developments; Jan 2018; DLRCC

****In accordance with Table 4.1 Cycle Parking for Residential Development, Standards for Cycle Parking and associated Cycling Facilities for New Developments; Jan 2018; DLRCC; 1 per 100m² GFA

6 no. cargo spaces shown; 5.45% of total

MOTORCYCLE PARKING PROVISION

Location of Spaces-

Undercroft (Total)	**2**

*Basis- In accordance with paragraph 12.4.7 DLR Development Plan 2022-2028- Minimum of 4 or more spaces per 100 car spaces

KEY APARTMENT FIGURES

Overall Schedule of Apartments and Mix-

LEVEL	1 BED	2 BED	3 BED	TOTAL
Upper Ground Level	4	3	1	8
Level 1	6	4	2	12
Level 2	6	4	2	12
Level 3	6	4	2	12
Level 4	6	4	2	12
Level 5	3	2	1	6
TOTAL	31	21	10	**62**
%	50.00%	33.87%	16.13%	100.00%
bedspaces	62	84	50	134

62

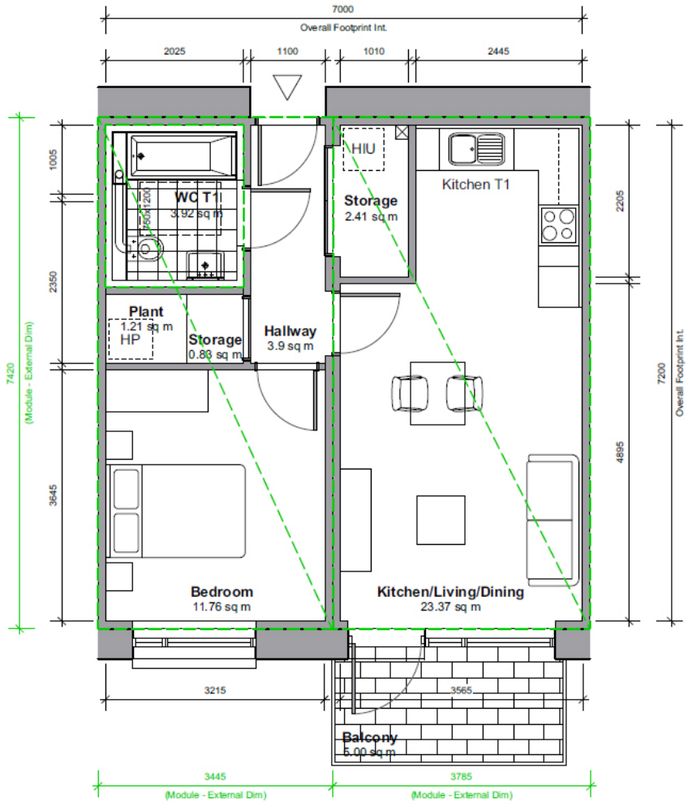


Image 21: Apartment Type A-1B2P-1 One Bedroom Apartment

8.0 Housing Quality Assessment

The scheme has been designed with due regard to the Sustainable Residential Development and Compact Settlements Guidelines for Planning Applications 2024, the Urban Design Manual Best Practice Guidelines, the Quality Housing for Sustainable Communities 2007, Dún Laoghaire-Rathdown County Development Plan 2022-2028 and the Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities 2022.

Space standards for the apartments are in accordance with the Sustainable Urban Housing - Design Standards for New Apartments 2018, Guidelines for Planning Authorities.

The apartments and associated common circulation areas have also been designed with due regard to the principles of universal design, including the 'Building for Everyone' publications. All units are Part M accessible, and 11 no. have been designed to be UD compliant.

A mix of unit types is proposed to cater for differing occupancy requirements.

One, Two and Three bed, units are proposed as follows:

1 Bed x 31 no.	(50%)
2 Bed x 21 no.	(33.87%)
3 Bed x 10 no.	(16.13%)

There are 4 apartment typologies proposed as schedules below.

50% of the units are dual aspect with the balance benefiting from either south, east, or west primary aspect.

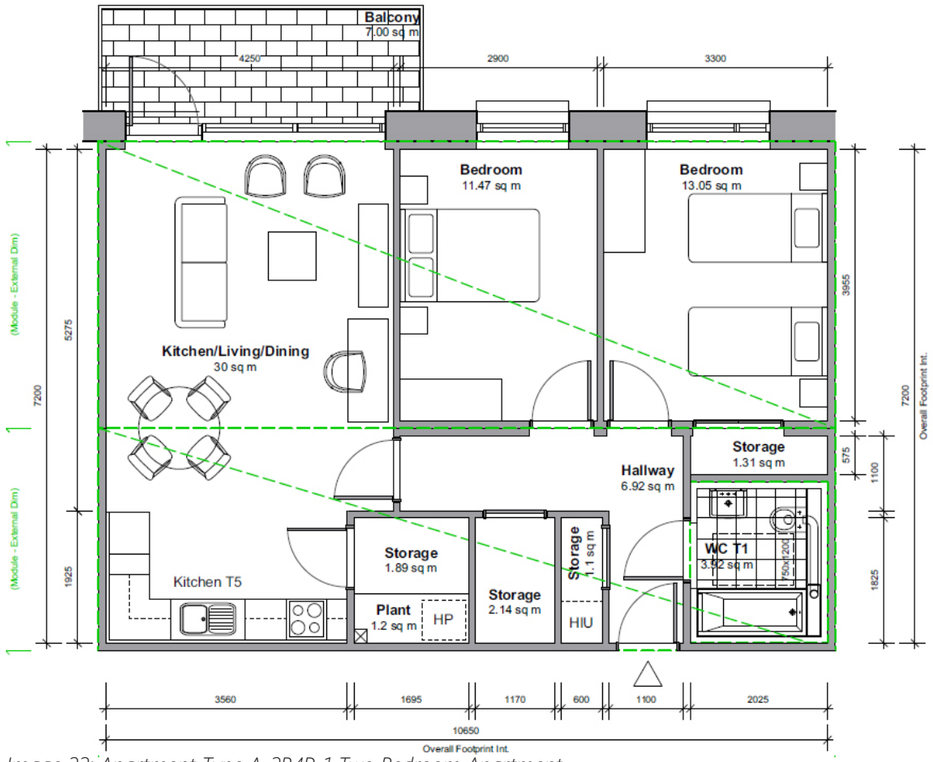


Image 22: Apartment Type A-2B4P-1 Two Bedroom Apartment

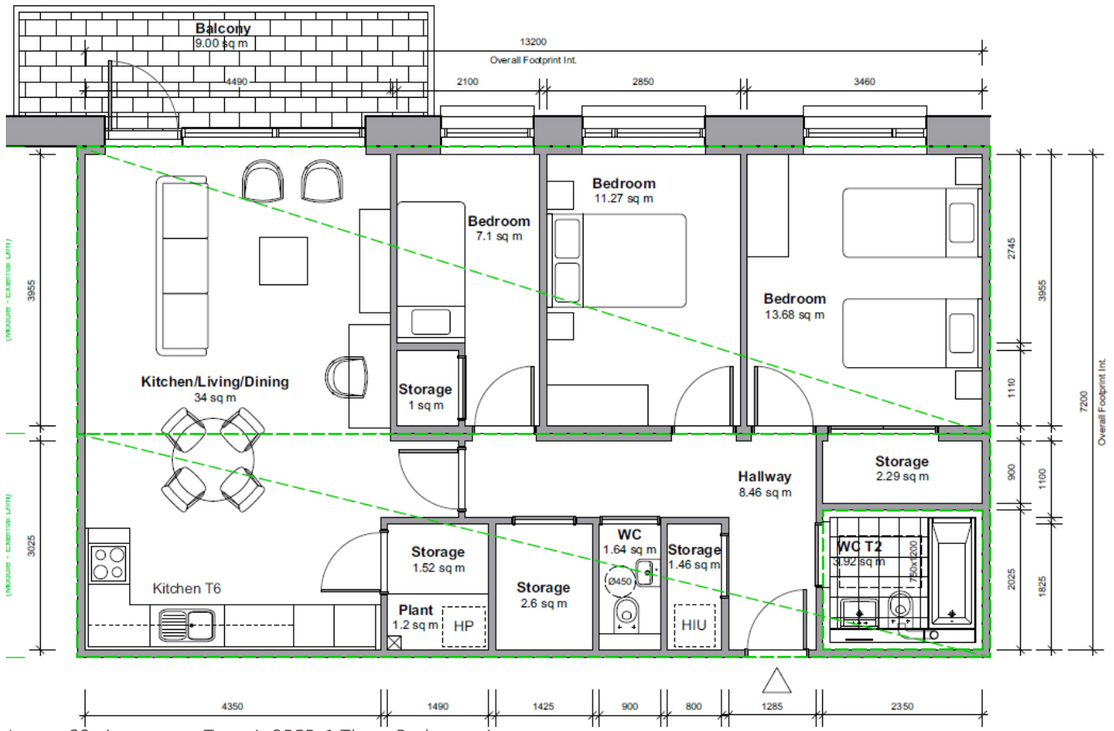


Image 23: Apartment Type A-3B5P-1 Three Bedroom Apartment

Apartments

Apartment areas, room areas and sizes are in accordance with Appendix 1 of Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities 2022. The schedules attached to this document itemise the apartments unit by unit and demonstrate compliance with the space, area and aspect standards. Room dimensions and areas are provided on the floor plans included as part of the application.

Storage is provided within each of the units as indicated on the drawings. Minimum storage areas of 3m² for 1-bed units, 6m² for 2-bed units and 9m² for 3-bed units . These areas consist of a mixture of dedicated utility/ storerooms, open shelving, additional wardrobe units and bathroom storage. In certain areas individual storerooms may be slightly larger than 3. 5sq.m due to the efficiencies of the plan layouts.

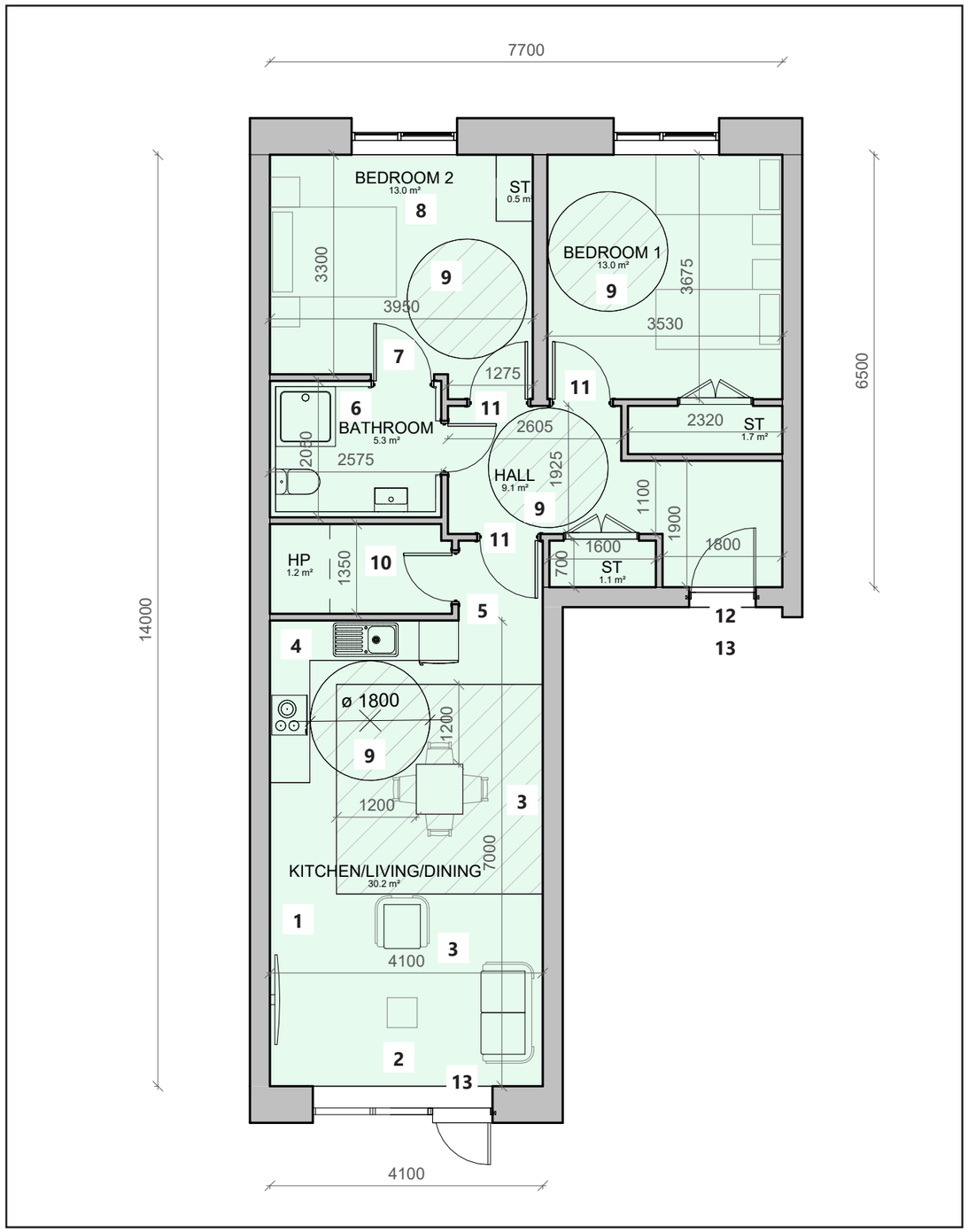
Each apartment has a balcony or terrace area accessed from the main living space. The minimum areas provided are 5m² for 1-bed units, 7m² for 2-bed units and 9m² for 3-bed units.. All balconies and terraces shall have a minimum depth of 1.5 sq.m.

Minimum aggregate floor areas for living/dining/kitchen rooms, and minimum widths for the main living/dining rooms		
Apartment type	**Width of living/ dining room**	**Aggregate floor area of living / dining / kitchen area**
One Bedroom	3.3m	23 sq.m
Minimum bedroom floor areas/ widths		
Type	**Minimum width**	**Minimum floor area**
Single bedroom	2.1m	7.1 sq.m
Double bedroom	2.8m	11.4 sq.m
Twin bedroom	2.8m	13.0 sq.m
Minimum aggregate bedroom floor areas		
One bedroom	11.4 sq.m	
Minimum storage space requirements		
One bedroom	3 sq.m	
Minimum floor areas for private amenity space		
One bedroom	5 sq.m	

Information taken from: Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities 2022.

2 bedroom Apartment Type A2B4P-5
(11 no. units)

Fig 24: UD provisions hatched



9.0 Accessibility

11 of the units have been designed to be UD compliant with the requirements of the 'Universal Design Guidelines for Homes in Ireland' developed by the Centre for Excellence in Universal Design (National Disability Authority). Accessible parking spaces are located nearby the units universally designed to provide easy access.

The Universal Design units consist of:
- 2 bedroom Apartment Type A2B4P-5 (11 units)

The criteria addressing the design of the internal unit layout are summarised as shown below.

1. Living/ Dining/ Kitchen; Large and flexible room with ample unobstructed space to access all areas for everyone with ease of movement thought the kitchen
2. Minimum 800mm wide clear route between furniture and in front of windows and routes between doors
3. 1200mm clear space on three consecutive sides of a table
4. Kitchen is not a thoroughfare. Cooker / hob and sink are in the same run of worktop.
5. The kitchen space located next to the dining area to ease access for carrying food and crockery.
6. Large and accessible bathroom (minimum 2100 mm x 2500 mm) door opens outwards, with level access shower.
7. Bathroom adjacent to the main bedroom with flexibility to provide direct access from the bedroom.
8. Clear access space of 800mm on both sides and at the end of the double bed.
9. Provide a clear space for a turning circle of 1500mm
10. Utility room with space for washer and dryer adjacent to kitchen.
11. Doors open into rooms (such as living rooms, bedrooms and kitchens) with hinge-side of the door is adjacent to a return wall and approx. 300mm clear space on the leading edge of doors
12. Entrance door with a clear width of circa 1000mm; with 1200 mm x 1200 mm clear landing.
13. Level access to the front door and balcony door

10.0 Building Lifecycle Analysis

A building lifecycle report is included with the application in response to Section 6.13 of 'Sustainable Housing: Design Standards for New Apartments'.

11.0 Energy Efficiency Strategy

All units are designed with an efficient plan format balancing the ratio between floor area and external wall which allows for an efficient and sustainable layout while also creating an efficient thermal envelope. The units will be constructed to building regulation standards delivering a high level of energy efficiency in use in line with Part L's near zero energy target. A Climate Action, Sustainability and Part C compliance report has been prepared by Semple McKillop and is included with the application. Measures have been fully integrated into the design.

12.0 Community Safety

Passive surveillance measures have been fully integrated into the design, ensuring that public spaces are naturally overseen by adjacent dwellings. A continuous active street frontage is provided with dual aspect corners and gables.

Permeability is provided across the site between Blackthorn Drive to the south, cedar road to the north and the public open space to the east, promoting connectivity for pedestrians and avoiding a cul-de-sac arrangement.

Parking spaces are located in areas that are well overlooked and proximate to the residential cores. Well-illuminated streets and landscaped areas, coupled with a variety of open spaces, contribute to a vibrant and safe community atmosphere. Secure Bicycle storage and bin storage is provided within the undercroft area.

13.0 Summary

- In developing the design proposal for this site at Balally, MCORM have endeavoured to apply best practice architecture and urban principles.
- We have had due regard to the Dún Laoghaire-Rathdown County Development Plan 2022-2028, relevant national policy and guidance together with the feedback provided from the local authority during the pre-planning phase.
- Our design has considered the potential impacts and benefits of the proposal in accordance with the 12 criteria of the Urban Design Manual -at the scale of the neighbourhood, at the scale of the site, and at the scale of the dwelling. Our design seeks to respect the existing characteristics of site whilst also utilising the unique conditions presented.
- A scheme of 62 residential units and a community room is proposed together with associated site works, landscaping and public open spaces. The scale and massing is appropriate for its location while the adopted architectural treatment affords a simple but modern aesthetic for the new scheme, adding to the emerging urban nature and character of Balally.
- We submit that the proposed scheme will constitute sustainable residential development which delivers high-quality design and residential amenity on the subject site.

Image 25: View from west along Blackthorn Drive; Balally Shopping Cenrte on the left

Appendix A: Housing Quality Assessment

23004 - Balally		
SHB5-BDR-SH-MCO-AR-P1 - HOUSING QUALITY ASSESSMENT SCHEDULE	PRE PART 8 SUBMISSION	March 2024

MCORM ARCHITECTURE AND URBAN DESIGN

Bundles 4&5 - Balally, Dublin 16- Dun Laoghaire Rathdown County Council

Level	UNIT NUMBER	UNIT TYPE	DESCRIPTION	UNIT AREA (m²)	BEDROOMS	BEDSPACES	AGGREGATE LIVING/ DINING/ KITCHEN AREA (sqm)	Bed 1 (m²)	Bed 2 (m²)	Bed 3 (m²)	AGGERGATE BED AREA (m²)	STORAGE (m²)	PRIVATE AMENITY (m²)	ASPECT	ORIENTATION (single aspect units only)
Upper Ground	1	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	2	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	3	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	4	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	5	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	6	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	7	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	8	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
Level 01	9	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
	10	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	11	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	12	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	13	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	14	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	15	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	16	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	17	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	18	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	19	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	20	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
Level 02	21	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
	22	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	23	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	24	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	25	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	26	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	27	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	28	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	29	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	30	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	31	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	32	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
Level 03	33	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
	34	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	35	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	36	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	37	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	38	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	39	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	40	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	41	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	42	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	43	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	44	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	

MCORM ARCHITECTURE AND URBAN DESIGN

Appendix A: Housing Quality Assessment (Continued)

Level 04	45	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
	46	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	47	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	48	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	49	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	50	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	51	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	52	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	53	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	54	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	55	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	56	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
Level 05	57	Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5	0	25.1	6	7	Dual Aspect	
	58	Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	Dual Aspect	
	59	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	60	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	West
	61	Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7	0	0	11.7	3	5	Single Aspect	East
	62	Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6	0	28	6.2	7.2	Dual Aspect	
	TOTAL NET AREA- RESIDENTIAL			**4146.11**	**m²**										

Appendix B: Summary of Residential Mix

23004 - Balally

SHB5-BDR-SH-MCO-AR-P1 - HOUSING QUALITY ASSESSMENT- SUMMARY OF RESIDENTIAL MIX **PRE PART 8 SUBMISSION**

M'CORM ARCHITECTURE AND URBAN DESIGN

Bundles 4&5 - Balally, Dublin 16- Dun Laoghaire Rathdown County Council

UNIT TYPE	DESCRIPTION	UNIT AREA	BEDROOMS	BEDSPACES	AGGREGATE LIVING/ DINING/ KITCHEN AREA (sqm)	Bed 1 (sqm)	Bed 2 (sqm)	Bed 3 (sqm)	AGGERGATE BED AREA (sqm)	STORAGE (sqm)	PRIVATE AMENITY (sqm)	QUANTITY	MIX	%
Apartment Type A-1B2P-1	Apartment	50.4	1	2	23.1	11.7			11.7	3	5	31	31	50.00%
Apartment Type A-2B4P-1	Apartment	77.76	2	4	30	13.6	11.5		25.1	6	7	10	21	33.87%
Apartment Type A-2B4P-5	Apartment	80.41	2	4	30.1	15.4	12.6		28	6.2	7.2	11		
Apartment Type A-3B5P-1	Apartment	92.16	3	5	34.5	13	11.3	7.3	31.6	8.9	9	10	10	16.13%
APARTMENT TYPES TOTAL												62		100.00%

M'CORM ARCHITECTURE AND URBAN DESIGN

Image 26: View from west along Blackthorn Drive; Balally Shopping Cenrte on the left

Appendix C: Design Checklist

The following is a tabulated response to the items listed in Appendix D - Design Checklist (Key Indicators of Quality Urban Design and Placemaking) of the Sustainable Residential Development and Compact Settlements Guidelines for Planning Applications. This design report details the design approach for the scheme under the headings as listed on the contents page. This Appendix is should be read in conjunction with the details set out in the main body of the report

No.	Topic	Response
1	**Sustainable and Efficient Movement**	
(i)	permeable and legible network of streets and spaces within the site	A clear urban design strategy is proposed consisting of a north-south pedestrian and cycle route connecting Blackthorn Drive to Cedar Road and the public open space to the east.
(ii)	connections with and between established communities, services and other uses	The site is located immediately to the east of Balally Shopping Centre and is directly accessible form here. The site is fully permable and creates connnections between established residneital communities north and south of Blackthorn Drive.
(iii)	streets designed in accordance with DMURS	Vehicular access is limited to the car parking spaces to the north and in the undercroft with a sinle entry/ exit opint from Cedar Road. The remainder of the site is designed purely for pedestrian and cyclist accessibility.
(iv)	quantum of parking been minimised	Parking has been provided at a rate of 0.387 spaces (a total of 24 no.) per residential unit. 10 parking bays are provided on street in permeable paved bays and a further 15 bays are discreetly located within the undercroft.

Image 27: View from Drummartin Link Road to the east at its interseection with Blackthorn Drive

Appendix C: Design Checklist (Continued)

2	**Mix of Land Uses (Vibrant Centres and Communities)**	
(i)	mix and intensity of land uses appropriate to the site	The site is located immediately adjacent to Balally Shopping Centre on Neighbourhood Centre zoned lands. The uses proposed within the development, residential and community, shall add to and complement the mix of uses already available in Balally Shopping Centre.
(ii)	diverse and varied range of housing types	The scheme has been developed in accordance with the Dún Laoghaire-Rathdown County Council brief which is based on the social housing need demand for the area. 4 dwelling typologies are proposed including 1 bed. 2 bed, 3 bed apartments within the block.
(iii)	support the regeneration and revitalisation of an existing centre or neighbourhood	The existing site is greenfield within a suburban context which is undergoing a change into a more urban area. This scheme will contribute to the evolution of the general area in that regard, and will add to the growing critical mass to make the neighbourhood a success.
(iv)	enhancement of the public realm	As part of the scheme design is the layout of quality public realm around its perimeter, particularly facing Blackthorn Drive, in the form of a mini-civic plaza at the main entrance, quality finishes along the new pedestrian/ cycle route to the east and new tree planting to the north.
3	**Green and Blue Infrastructure (Open Space, Landscape and Heritage)**	
(i)	positively responded to natural features & landscape character	The form of the new building follows the natural fall across the site from south to north of approx. 3.7 metres, taking advantage of this fall by locating ancillary uses into an undercroft at lower ground level reducing its visual impact. There is tree planting proposed to compensate for the loss or removal of existing tees which are either of poor condition or cannot be retained along the western and northern boundary.

Image 28: View from Drummartin Link Road to the east at its interseection with Blackthorn Drive

Appendix C: Design Checklist (Continued)

(ii)	a complementary and interconnected range of open spaces, corridors and planted/ landscaped areas	The site is infill by its nature and is occupied predominately by the proposed apartment block. The public realm to the edges will provide a quality interface between the building and the large area of existing public open space to the east, forming a distinct and definable urban edge to this parkland. The planting in the communal garden to the west of the block will be augmented by new landscaping in the form of natural woodland ground cover and tree planting.
(iii)	public open spaces universally accessible and designed to cater for a range of active and passive recreational uses	The public realm that will wrap around the south, east and northern facades of the building will be fully accessible, notwithstanding the challenges presented by the change of level across the site. The new pedestrian/cycle link is designed at appropriate falls, and entrances to the apartments and community room are fully compliant with Part M requirements.
(iv)	integrated nature-based solutions for the management of urban drainage	The landscaping design incorporates permeable paving, tree pits and swales. Given the restricted space available on the site an drainage is attenuated below ground before discharge to the public system.
4	**Responsive Built Form**	
(i)	coherent and legible urban structure in terms of block layouts and building heights	A simple and clear urban block is proposed for the site, running in a linear formation along the eastern edge of the site. This block will provide a strong urban edge facing the open space to the east. Short returns at both the north and south create strong urban corners and contain the more private open space at the rear- defining public and private realm.
(ii)	buildings address streets and spaces	The 6 storey block fully overlooks the public realm on all sides and creates a new urban edge, where there was previously a very weak interface between the shopping centre and Blackthorn Drive and parkland at this location.
(iii)	layout, scale and design features of new development respond to prevailing development patterns (where relevant)	The development is appropriate for the creation of a new urban centre for Balally at this location. There is an evolution in the general vicinity particularly in Sandyford Business District from what was a car based business park and suburban environment in both scale and density to a more urban neighbourhood.
(iv)	coherent architectural and urban design	A six storey scheme is proposed with simple and robust corners and details to create a new urban composition. This together with the consistent palette of materials and finishes used across the scheme will create a coherent character within the development and in the context of the surrounding area.

TABLE 1- BICYCLE PARKING PROVISION		
Location	**No.**	**Note**
Undercroft (Secure Long Stay- for Residents)	108	Basis- 1 long stay space per bedroom, In accordance with SPRR 4 - Cycle and Storage, Sustainable Residential Development and Compact Settlements Guidelines for Local Authorities 2024 (6 Cargo Spaces indicated)
Undercroft (Secure Long Stay- for Comunity Room Staff)	2	Basis- based on Other Uses Not in Table- Minimum 2 In accordance with Table 4.2 Cycle Parking for Residential Development, Standards for Cycle Parking and associated Cycling Facilities for New Developments; Jan 2018; DLRCC
Surface (Short Stay Visitor for Community Room and Residential)	16	
Total	126	

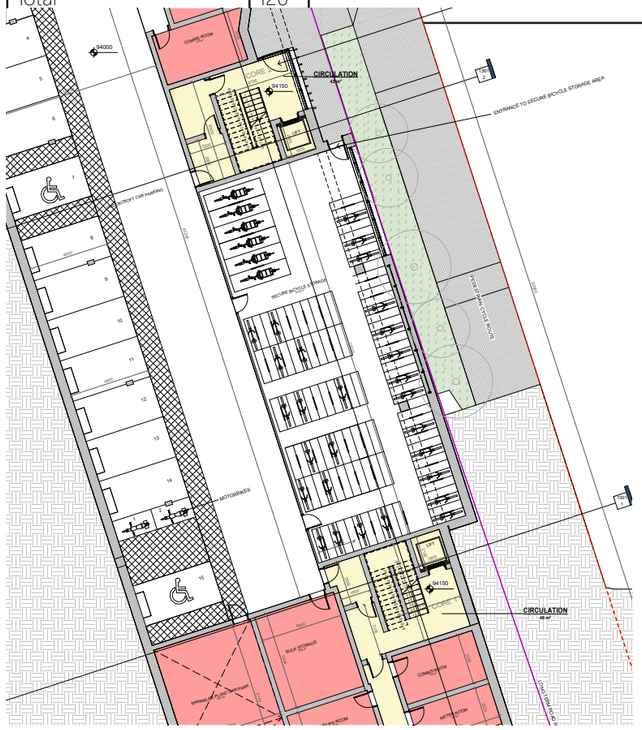


Image 29:
Undercroft Secure Bicycle Parking - Lower Ground Level

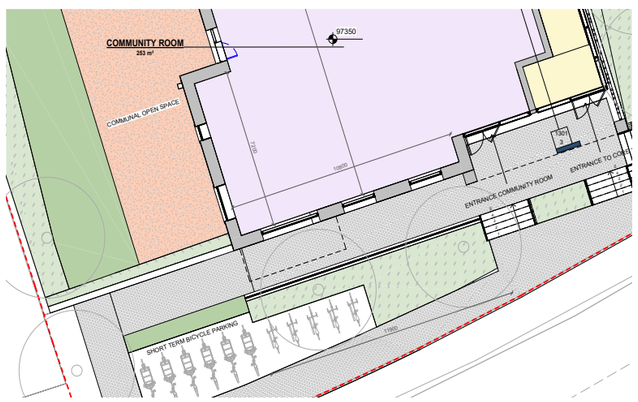


Image 30:
Short Stay Bicycle Parking - Upper Ground Level near entrance

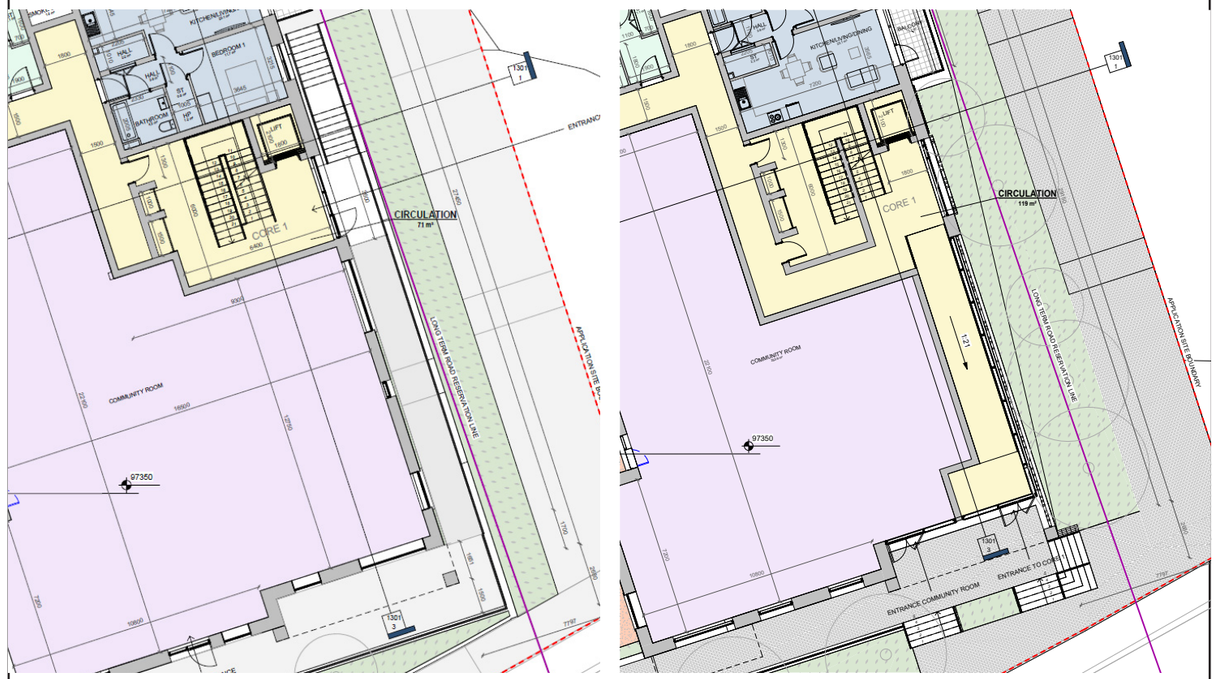
Appendix D: Cycle Audit

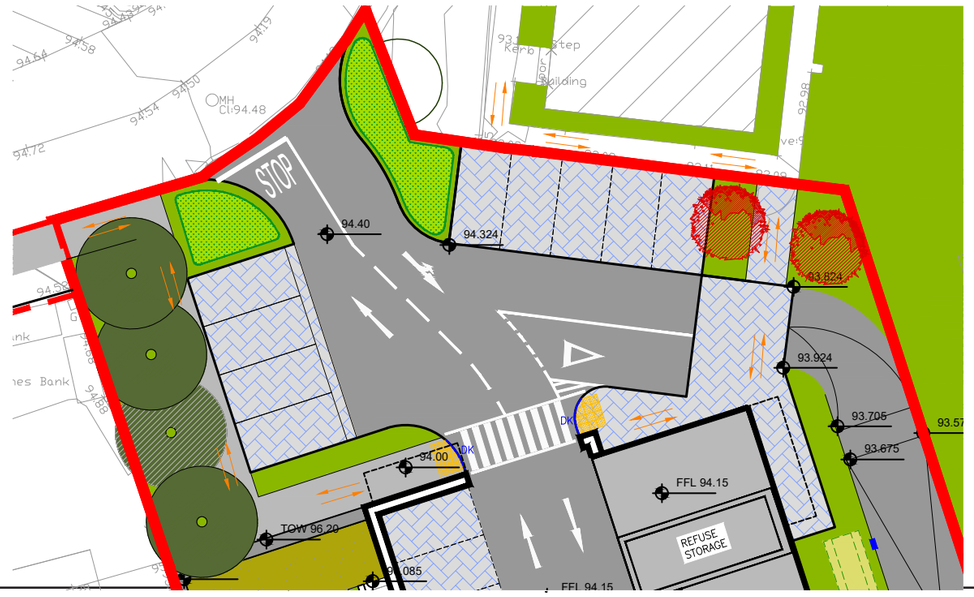
The following is a tabulated response to the items listed in Section 12.4.6.2 Cycle Parking Assessment Criteria, Dún Laoghaire-Rathdown County Development Plan 2022-2028.

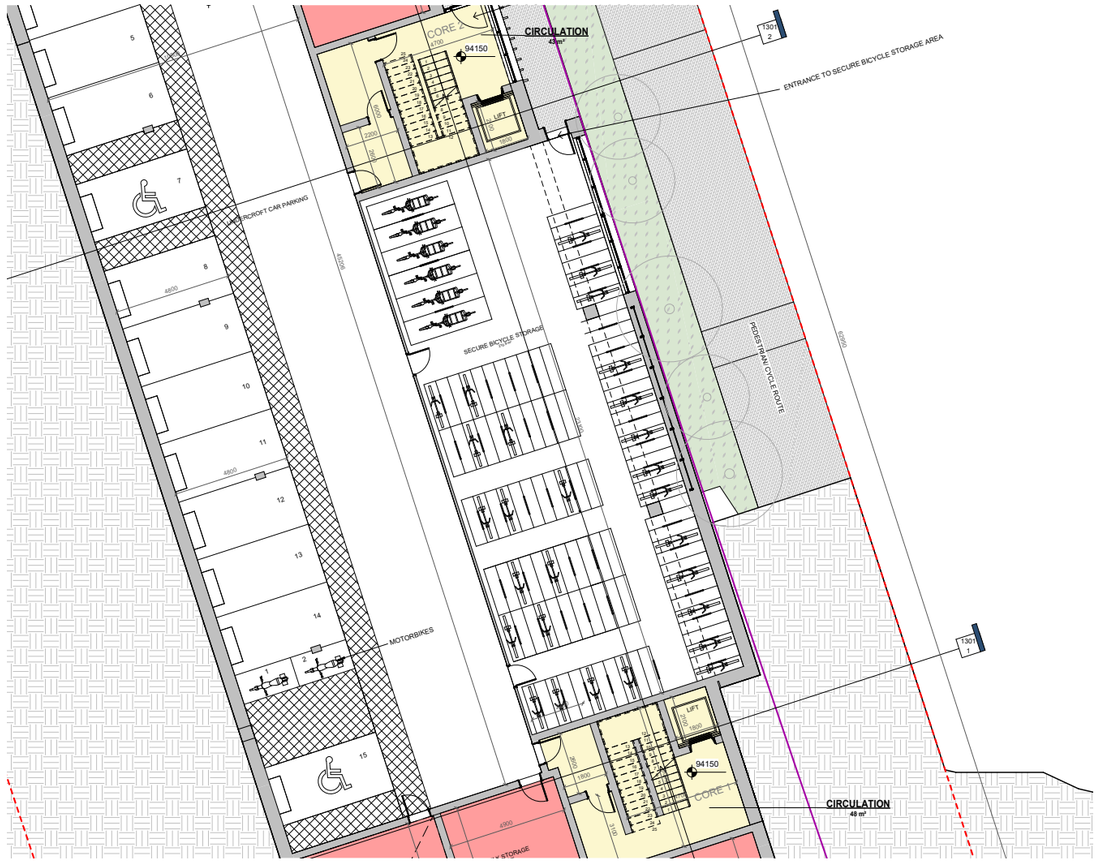
The development plan (under 12.4.6.1 Requirements for New Development) states-
"It is a requirement that, new residential developments of 5 residential units or more or non-residential type developments of 400 sq.m. or over, submit a Cycle Audit as part of the planning application. The Cycle Audit must be prepared by a suitably qualified person and shall clearly demonstrate, in plan format, how all the requirements of Council's Standards for Cycle Parking and Associated Cycling Facilities for New Developments, are met within the development."

This design report details the design approach for the scheme under the headings as listed on the contents page. This Appendix is should be read in conjunction with the details set out in the main body of the report

Criterion	**Response**
Is the number of cycle parking spaces and footprint adequate and is there suitable provision for parking of outsized formats (cargo bikes etc)?	Table 1 opposite sets out the proposed provision of bicycles in the development and the basis for calculation
Is the location of cycle parking convenient, appropriate and secure with adequate provision for covered parking?	Secure bicycle parking is provided in the undercroft, with a dedicated access point from the east facade connecting to the pedestrian/ cycle route, thus avoiding any conflict with vehicular traffic. See lower ground floor plan opposite.
Is the cycle parking area accessible in terms of dedicated access routes with ramps and/or kerb dishing where required?	Access to the secure bicycle parking is at grade via a dedicated doorway on the eastern facade directly from the Pedestrian/ Cycle route alongside the block. This is separate from the vehicular entrance to the undercroft.
Do the internal cycle access routes connect well with off-site cycle facilities – existing and proposed?	The cycle storage in the undercroft is directly accessible from the dedicated pedestrian/ cycle route outside. This in turn connects directly to the nearby cycle infrastructure on Blackthorn Drive.
Is there adequate and appropriately designed and integrated provision for ancillary cycling and pedestrian facilities including showers, locker / changing rooms and drying areas?	It is envisaged that as the cycle parking bays are for the residents, that this is provided for in each of the apartments.

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
1	Capital Projects have concerns about proposed use of part of strategic reservation in advance of outcome of study for use of corridor, DLR CDP SLO 4. (DLR tender for consultants to progress study expected this month). The following are more details on the encroachment on the strategic reservation. Noted the HRA Planning Statement Feb 2024 for Proposed Part 8 Residential Development Balally Sandyford includes under Chapter 9 Local Policy Section 9.8 Dublin Eastern-Bypass Reservation Corridor. This section 9.8 acknowledges DLR CDP Specific Local Objective No. 4 and DLR CDP Section 12.4.16 and states 'No buildings or structures are proposed within the area of the site located within the road reservation corridor, thereby ensuring future development of the land is not compromised.' However the MCORM Proposed Site Plan Drawing MCORM-AR-P3-1002 in addition to identifying eastern pedestrian cycle route shows some of proposed development building or structures within the strategic reservation corridor.	A flight of steps in the public realm is indicated and can be pulled back in behind the reservation line, indicated as a purple line on drawing no. SHB5-BDR-DR-MCORM-AR-P3-1101 (Upper Ground Floor Plan). Extract below-  These have been subsequently removed as part of the redesign of the entrance (upper right image); the area is now proposed as a planted buffer area

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
2	Capital Projects recommend further consideration of northern boundary treatment of proposed development at Cedar Road to ensure adjoining public realm prioritises all levels of pedestrian and cyclists across the proposed vehicular and service access at Cedar Road. This may include ensuring a continuous safe delineated footway/cycle way connecting route to Kilmacud Luas from Blackthorn Drive development frontage. Currently there appears to be no footpaths in vicinity of proposed Cedar Road access or existing scout hall.	The proposal on MOR Drawing is provided to achieve a safe entrance exit / entrance to the site for pedestrians to the East and to the West of the site. The complication which prevents the simple agreement with the requirements of DLRCoCo is that there is currently no footpath along the South side of Cedar Road. More importantly there is a major level difference in the ground levels along Cedar Road immediately East of the Site which makes it impractical to lay a footpath to the East of the site i.e. along the front of the Scout Hall. Hence it is proposed that a safe route for pedestrians is along the South side of the Scout Hall building which would then enter the Site near the North -East corner of the building- thereby removing pedestrians from the East site of the Site entrance. On the West side of the entrance, the safe access / exit for pedestrians would be via the footpath along the South side of the Cedar Road and the directly South between the proposed trees and the car parking spaces and safely to the pedestrian crossing along the North side of the proposed buildings 

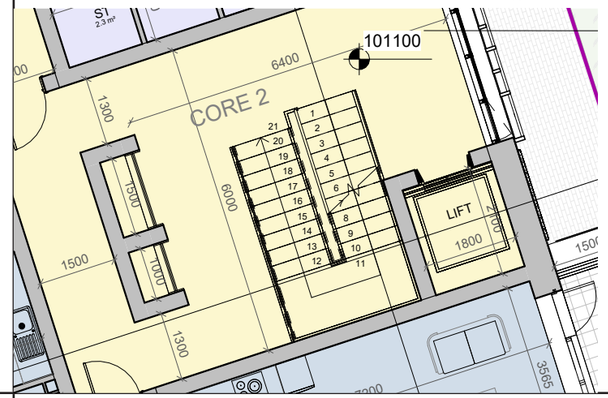
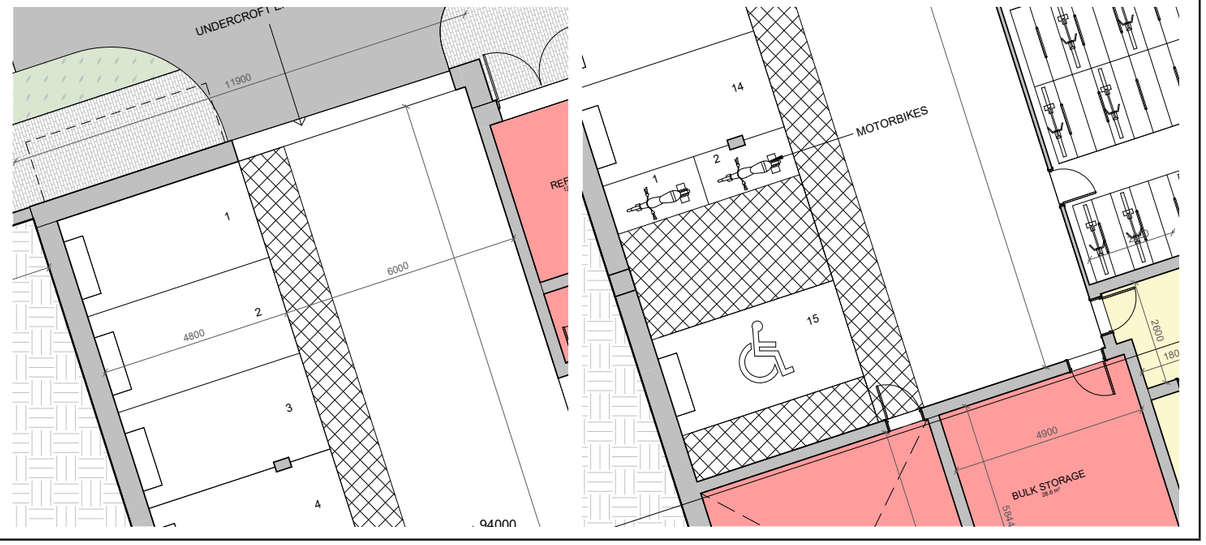
	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
3	The provision of cargo bike parking to the front of the development is noted and welcomed. Noted that provision is also made within the secure undercroft area for cargo bikes. The aisle widths and door opening widths may need to increase in accordance with DLRCC standards for cycle parking (Page 5)	The layout of the secure cycle storage area is designed to include the following, in line with figure 3.3 "Indicative cycle parking layout", Standards for Cycle Parking" (DLR-2018)- Standard cycle space- 0.5m x 2.0m Cargo cycle space- 1.0m x 2.5m minimum Clearance areas vary between 1.3m-1.6m between standard spaces and 2.0-2.7m for cargo bikes. A 1.0m wide access door is provided to the storage area from outside 

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
4	Drainage- The applicant has proposed Cv values of 0.84 for Winter and 0.75 for Summer however reduced runoff rates have been proposed. Appendix 7: Sustainable Drainage System of the County Development Plan 2022-2028 requires that if the applicant proposes to use their own reduced run-off rates, then the default Cv values should be amended to a value of 1.0. Maintaining the default Cv values in conjunction with reduced run-off rates for contributing areas reduces the surface water run-off during hydraulic simulations, resulting in inaccurate simulation results which may lead to under sizing of the drainage system and attenuation storage required. The applicant is requested to clarify if reduced run-off rates for contributing areas have been applied and if so to update the surface water management strategy/calculations as required.	The Cv values used in the Causeway Drainage Software will be amended to a value of 1.0 to comply with Appendix 7 of the Sustainable Drainage System of the County Development Plan because we are proposing to use our own reduced run-off rates.
5	Drainage- The applicant has proposed Cv values of 0.84 for Winter and 0.75 for Summer however, the applicant has used a Soil Type 1, which suggests good infiltration, however one infiltration test failed to produce a result due to failure of the water to infiltrate. The applicant is requested to revise the allowable outflow calculation for the site based on site specific data including soil type and SAAR. The system shall not have an overflow unless deemed necessary by soil infiltration testing results. Any overflow shall be limited to QBAR (calculated using site specific data) or 2l/s/ha, whichever is greater, subject to the orifice size of the flow control device not being less than 50mm in diameter. Note that in the interest of clarity where the calculated QBAR rate for the site is less than 2 l/s/ha then a minimum value of 2 l/s/ha should be applied, not a flat rate of 2 l/s, i.e. the outfall discharge rate should be calculated based on the lowest flow rate achievable for a 50mm Unit Outlet Diameter on the proposed flow control device using an appropriate method such as the Hydro International online Optimum Design Tool. This may result in a change to the attenuation volumes required. Drainage would still say Soil Type 2 is incorrect for the area. Further review to take place when the detailed design comes in. The Soil Type should be based on site investigation.	The Soil Type will be changed to Soil Type 2 due to the result of the site investigation. The outfall discharge rate has been recalculated with the correct QBAR - in this case the calculated QBAR Rate for the site is not less than 2l/s/ha therefore we will use the calculated outfall discharge rate for the site.
6	Drainage- The applicant has analysed the proposed drainage network with a limited number of storm durations. For completeness, all storm water durations, up to an including 10080 mins should be analysed.	The proposed drainage network will be amended to analyse for all storm water durations up to 10080 mins.

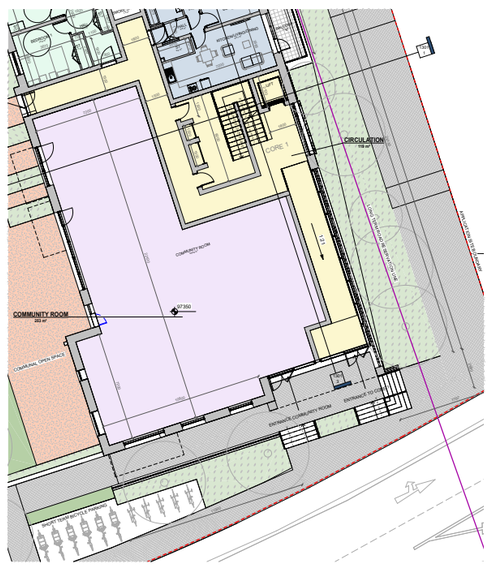
	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
7	Drainage- Prior to commencement of development the applicant shall submit to the Planning Authority for its written agreement full details of the blue roof drainage network, with outfall to the public network. The drawings should clearly indicate if the roof drainage goes into the drainage network on the ground, and the proposed tank and if so, has this flow been considered in the design of the attenuation tank. Note a separate discharge may be possible, however the allowable outflow for the entire site should be limited to Qbar or 2l/s/ha, as standard.	The engineering report will be updated to include a clear statement on the blue roof drainage. The proposal is that a percentage of the overall roof will be a green roof underlaid by a storage medium so that it also performs as a blue roof, capable of attenuating rainwater. The proposed green/blue roofs will provide initial storage of rainwater, while also reducing the rate at which rainwater from heavier rainfall events discharges to the attenuation system. Flow restrictor outlets will be provided to control the rate of runoff from the roof. Since the green / blue roofs provide their own attenuation with flow restrictor outlet on the roof, these areas will not drain towards the main attenuation tank located. Runoff from the green / blue roofs will connect to the surface water drainage pipework downstream of the main attenuation tank and associated Hydrobrake.
8	Drainage- Prior to the commencement of development, the applicant shall submit details of the Green Roof to the Planning Authority for its written agreement. The applicant is requested to provide a detailed cross section of the proposed build-up of the green roof, including dimensions and demonstrate that the green roof is designed in accordance with BS EN 12056-3:200 and The SUDS Manual (CIRIA C753). A construction plan and a post-construction maintenance specification and schedule should also be included.	Noted. Drawings will be revised to capture item raised.
9	Drainage- Prior to the commencement of development, the applicant shall submit details of The applicant shall ensure that trees shall not be planted in the area over the attenuation tank. Trees shall be placed at a minimum distance of 2m from the edge of attenuation tanks. Tree protection barriers may be required, depending on the tree species and the expected extent of root spread, to be advised by the landscape architect.	Noted. Drawings will be revised to capture item raised.
10	Drainage-he applicant shall ensure that all drainage works are carried out in accordance with the agreed details and that a post-construction maintenance specification and schedule is implemented on site. Maintenance contractors with specialist training in SuDS care should be used. Thereafter, all elements of the surface water management system shall be maintained at all times in accordance the post-construction maintenance specification and schedule, which shall be included in the site Safety File.	Noted.

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
11	Drainage- Prior to the surface water connection to the public system, the applicant shall make a submission for the written agreement of the Planning Authority, showing that the attenuation system, including the flow control device, has been installed according to the planning application plans and conditions, and set to the maximum permitted discharge limit. This shall include photo documentation of the installation process, and certification from who installed the system. The applicant shall then facilitate an inspection from the Planning Authority and will proceed to connection if the inspection was deemed satisfactory.	Noted.
12	Biodiversity- All recommendations of the Ecological Impact Assessment will be implemented. Reason: For the enhancement and benefit of biodiversity and align with National and County policies.	Noted.
13	Biodiversity- No vegetation clearance will take place during the bird breeding season (March 1st – Sept 1st) Reason: To protect breeding birds during the bird breeding season.	Noted.
14	Biodiversity- A Final Landscape Plan and species lists will be agreed with DLR Parks and Biodiversity Officer prior to commencement of the proposed development and will provide: a) The inclusion of bird bricks, boxes and callers including for swifts will be provided in the Final Landscape Plan with input from a suitably qualified ecologist in consultation with DLR Biodiversity Officer. b) The inclusion of a hedgerow with diverse native species will be detailed in the Final Landscape Plan with sufficient buffer and space from hardstanding areas, in consultation with DLR Parks and Biodiversity Officer. c) Green roof areas will be examined for the provision of biodiversity elements where feasible. Reason: To ensure the appropriate design of the landscape plan including for biodiversity.	Noted.
15	Biodiversity-Lighting will be designed and provided to avoid light spill and light pollution to the landscaped areas of trees and/or hedgerow. Reason: To ensure that the newly created landscaped areas in particular those for biodiversity are not impacted by light pollution.	Noted- Final lighting design and layout drawing to form part of the final package of documents

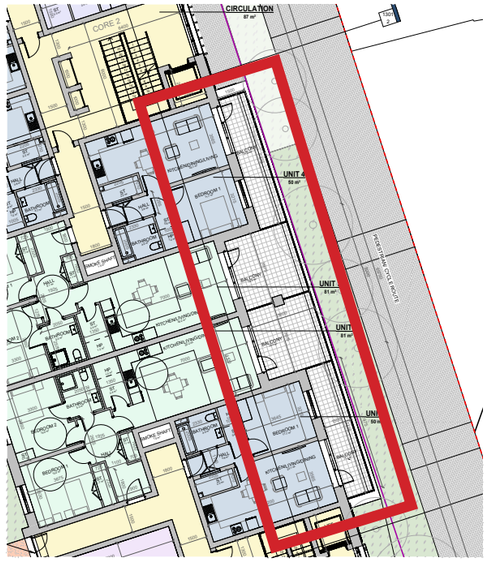
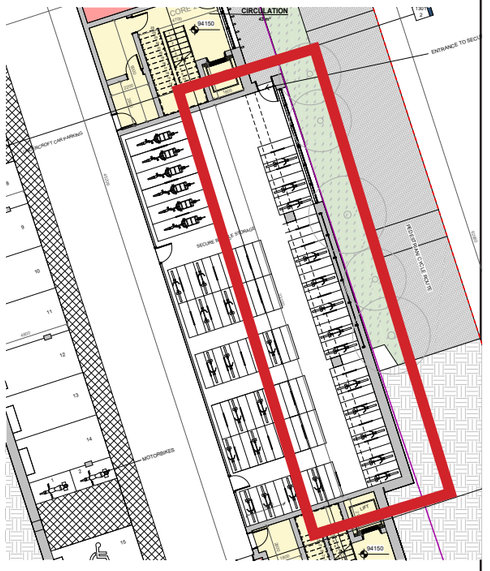
	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
16	Architects- Sustainability Report- Not specific to the development - mentions housing / apartments and duplexes Need to know specifics for detail design in relation to strategies, heat & ventilation, a number are mentioned, generic in content.	This has been updated to remove any reference to Housing and reissued to Asite. We cannot be more specific as this is a PPP development and PPP Co. are to be given some flexibility in terms of their bid proposals.
17	Architects- Creche- Does this have a tenant? Query size of the creche and is it fit for purpose in relation to the occupier and their model / how is this being funded in relation to social housing? DLR Housing to confirm requirement for crèche in the project brief	Creche is no longer being included in scheme as agreed with DLR Housing. This is now shown as a flexible Community Space which can be fitted out at a later stage.
18	Architects- DLR County Age Friendly Strategy – Universal Design Homes DLR Housing to confirm requirement for provision of UD and Lifetime Home in the project brief is in line with DLR Age Friendly Strategy 2022-2026.	Confirmed as per email from DLR Housing (06/06/2024) that "DLR are happy to proceed with 18% UD at this location".
19	Architects- UD Homes Compliance / General – Apartment Layout Universal Design Guidelines not achieved: -Ensure that double and twin bedrooms are at least 13m² in area. -Provide entrance hallways with a space of between 1500 × 1500mm and 1800 × 1800mm adjacent to the entrance door. -Design the kitchen/ counter layout in a 'U' or 'L'-shape. -Bathroom minimum internal dimensions of 2100 x 2400mm (plans indicate it is 2000 x 2200mm). The plan arrangement not in keeping with Indicative Shower room layout It is worth noting that the apartment layout shown in Figure 24 of the Architectural Design Statement is not the layout shown on the floor plans. The layout shown on the floor plans is even further from meeting the UD Homes guidelines.	Noted- The layouts shall be amended to include the updated 2-bedroom UD apartment type being proposed across the bundle. Also Updated in Design Statement. 

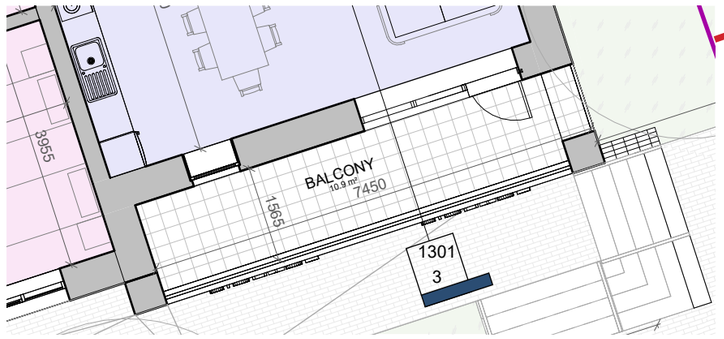
	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
20	Architects-The passenger lifts designed to facilitate access to these Universal Designed Apartments have been undersized. They are shown with a shaft size of 1800 x 1800mm. UD Homes guidelines require a minimum of one passenger lift of minimum dimensions 1525 × 2030mm in four and five storey buildings provide. A lift of these dimensions' minimum must be provided along the proposed circulation routes to the proposed UD Apartments.	Noted- This increased lift size can be easily accommodated and has been incorporated into the final layout. Internal clearance, 1800 x 2100mm as indicated on drawings 
21	Architects-The accessible parking spaces shown on the Lower Ground Level plan do not comply with Irish Building Regulations. The spaces are dimensioned at 4600mm in length and no 1200mm access zone has been identified. There is no safe pedestrian route leading from the accessible spaces to the circulation cores. In addition to this, no provision has been made on the plans for structure in the parking area, which will further restrict parking widths. No turning head has been provided at the end of the vehicular circulation.	Noted- The dimension should read 4800mm. The final layout shall indicate markings around the accessible parking spaces and a route to the 2 circulation core entrances. It is proposed that the last parking space in the undercroft parking be removed and replaced with a hatched area on the ground to serve as a turning area as shown on the Lower Ground Floor Plan. 

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
22	Architects- For pedestrian circulation, the ramped approaches to the apartment cores are quite long and convoluted and located around the side of the building. Additionally, the entrances to the building are not readily apparent. Navigability will be challenging for those who are visually impaired or with memory impairment. The UD Homes Guidelines call for the layout of the buildings, roads and streets, and position of signage in a new development should make it easy for everyone to find their way around. In the view provided, taken from Blackthorn Drive and the Drummartin Link Road, the proposed entrances to the main stair cores have little or no presence. Similarly, when looking at the southern elevation there is no indication as to where the approach to the main stair core is located. Site navigability would be challenging to find for those visiting the building, whether or not they had and impairment to their sight. Providing signage/ additional features to augment the presence of the entrances seems a less than ideal solution to this issue. Please provide details of what features you propose along with potential amendments to the design to improve navigability and wayfinding in the Development.  	The CGIs shown form part of the verified views pack, and are to primarily illustrate the visual impact on its context. Therefore, the northern entrance is located behind the existing fence and hedge in the foreground. We have reviewed both the approaches and entrances to the 2 main residential cores into the building as follows- Core 1 South has been reconfigured so that the main entrance point is now at the front of the block adjacent to an updated entrance into the Community Room. This caters for Part M accessibility as there is an internal slope that brings you from the door to the upper level where the lift and stair is located. This route is provided with full height fenestration along the eastern facade of the block allowing generous penetration of natural light into this entrance space. This has been achievable by the removal of the proposed creche, allowing internal space to be reconfigured to allow this access from the front. Accordingly, the access route that ran along the eastern facade of the building has been removed as well as the external steps. The front flight of steps is now repositioned, centred on the main entrances to the south elevation providing greater legibility. Core 2 north is accessed at the lower ground floor level given the fall across the site. We have created a splayed wall effect which creates a sheltered porch area externall to this entrance with textured brickwork on the play was well as natural stone above the entrance in the same finish as core 1. This area asl provides the access point for cyclists into the secure storage space 

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
23	It is very difficult to determine what the accessible route is on the Ground floor plan; see below. The ramp seems circuitous; with the starting point of the ramp being visually divorced and physically remote from the entrance to the building. We would be concerned that this approach is not in compliance with Universal Design Principles, which note: "Wherever possible, the top and bottom of a ramp should be adjacent to the top and bottom of an associated flight of steps. The location of stepped and ramped routes should be clearly obvious. Where steps and ramps are provided to gain access to a building entrance, they should both be clearly visible from the approach route."1 Similarly, the Irish Wheelchair Association guidelines state: The route to the principal entrance of a building, from all areas, including the car parking area, should be clearly identified and well-lit [...] The steps and ramps should be as close as possible to each other."2 1 Building for Everyone: A Universal Design Approach 2 Best Practice Access Guidelines 4-Designing Accessible Environments – Irish Wheelchair Association  	See response above with updated layout at this location. Further information below to explain the revised entrances; Left- Upper Level Plan showing new entrance to southern core directly from southern facade paired with new entrance to community room within projecting frame feature. External path and steps on eastern facade removed. Right- Lower ground Level Plan entrance to Core 2 and cycle storage. Below, 3d massing model to illustrate further these updates. 

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
24	Architects- Finally, it is noted in the Architects response that the eastern elevation of the building is considered to be one of the two principal elevations of the apartment block. Were this the case, we would have concerns over the quantum of dead frontage / Blank facade and building service areas proposed on this principal elevation. Though the Design Statement suggests "maximum passive supervision is provided" in actual fact there is very little engagement with the public realm at the lower ground floor level. There is therefore no direct passive observation of the pedestrian circulation route from the north/ Maples Road to the Apartment entrances. As with previous comments, it is also not clear from the elevation where the entrances to the building are.	See responses above regarding entrances and approach. The façade facing Maple/ Cedar Road to the north accommodates the main point of vehicular access to the undercroft as well as a portion of external parking. Also, there are access points for the ESB substation located at the corner. It is submitted that while there aren't apartments at lower ground floor level here, it will be an active area with residents parking and accessing vehicles. Additionally, there are 5 storeys of apartments (10 apartments in total) above overlooking this area.  The entrance at lower ground floor level with its splayed wall feature will be visible as one turns the corner from the northern façade as described above.

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
24 cont.	 	See responses above regarding entrances and approach. To enhance the supervision of the eastern flank of the block, we have created a plinth of enlarged terraces serving the upper ground level apartments bringing these closer to the sloped route running alongside the building. As well as increasing the private amenity spaces of these units, it also increases the area internally of the secure bicycle storage room below.  

	Following the first submission of the design pack on 01/03/2024, we have addressed the comments received from DLR as per the table below	
	DLR Comment	Design Team Response
25	Architects- It is noted the balconies proposed follow the minimum standards, notwithstanding this the balcony design should also look at location of development, useability of the balcony, as well as cost as noted. It appears that the recessed balcony to the front is more to help with the elevational treatment, however it would be beneficial if more of the apartments if not at least all the 3 bed units had the benefit of a recessed balcony.  	We have introduced a framed structure serving the 3-bedroom apartments at the northern end of the block facing east, which acts as a counterbalance to the south-facing frame serving these 3-bedroom apartments, which provide a sense of enclosure. It should be noted that the balconies serving the 3-bedroom units are 10.9m² each, more than the 9m² minimum. See Extract Plans on Left Side It should also be noted that we have recessed balconies serving the 11 no. D 2-bedroom units, as per the extract below-